



# PORT REGULATION OF THE PUBLIC PORTS IN SR

(Operation regulation of public ports in SR)

Instruction of the Operator of Public Ports in the Slovak Republic issued in accordance with Article 5 par. 5 and 8 of the Act No. 338/2000 Coll. on Inland Navigation and on Amendments to Certain Acts, as amended.

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# PORT REGULATION – PART ONE

## Article 1

### Introduction

- 1.1** This Port Regulation for Public Ports of the Slovak Republic (Operation Regulation of Public Ports of the SR) (hereinafter referred to as the “Ports Regulation”) issued by the company Verejné prístavy, a.s. with registered office at Prístavná 10, 821 09 Bratislava, Company ID: 36 856 541, in accordance with Article 5 par. 5 and 8 of Act no. 338/2000 Coll. on Inland Navigation and on Amendments to Certain Acts, as amended (hereinafter referred to as the “Act on Inland Navigation“), regulates:
- the conditions for inland navigation, the rights and obligations of legal and natural persons involved in inland navigation in the area of the public ports of the SR.
  - method of operation and use of the Danube public ports in Bratislava, Komárno and Štúrovo, unless otherwise agreed between the Public Port Operator and the users of public ports.
  - rights and obligations not specifically regulated by these Port Regulation are governed by the Inland Navigation Act and related generally binding legal regulations.

## Article 2

### Public Ports Operator

- 2.1** Verejné prístavy, a. s. is the operator of public ports with its registered seat at Prístavná 10, 821 09 Bratislava, Company ID: 36 856 541, registered in the Business Register of the District Court Bratislava I., Section Sa, Insert no. 4395/B (hereinafter “Operator” or “Company”). The operator is a joint-stock company pursuant to Article 154 et seq. of the Commercial Code with the specifics which are regulated by the Act on Inland Navigation and its only founder is the Slovak Republic, on behalf of which the Ministry of Transport and Construction of the Slovak Republic acts within the Company.
- 2.2** In accordance with the Inland Navigation Act, the operator is responsible for collecting payments for the use of public ports. The amount of the payment shall be determined by the Operator based on the Schedule of Payment charges for the use of public ports on the waterways of the Slovak Republic (hereinafter referred to as the “Schedule”) approved by the Ministry of Transport and Construction of the Slovak Republic, with the current wording published on the website of the Operator at [www.portslovakia.com](http://www.portslovakia.com).

## Article 3

### Waterways – administration and state supervision

- 3.1 The operator of the waterway (Article 3 par. 1 of the Inland Navigation Act) is the administrator of significant water-management watercourses or the administrator of the small watercourse, which in case of Danube river is the Slovenský vodohospodársky podnik, š.p. (Slovak Water Management Company).
- 3.2 The watercourse administrator, who is responsible for the waterway operation, is responsible for maintaining and improving the conditions of operation of the vessels on the monitored waterways. The watercourse administrator is responsible for the specification of the waterway, for the indication of navigation barriers on the waterways and for their removal based on the instructions of the Transport Authority.
- 3.3 Pursuant to Article 39 of the Inland Navigation Act and Act no. 402/2013 Coll. on the Office for Regulation of Electronic Communications and Postal Services and on the Transport Authority and on Amendments and Supplements to Certain Acts, as amended, the Transport Authority performs the activities of the State Professional Supervision for Inland Navigation (hereinafter referred to as “SPS”).

## Article 4

### Definitions

- 4.1 **Public port** is a defined area, including the water section, structures and facilities used for the transshipment, storage, processing and transport of goods, the embarkation and disembarkation of passengers and the protection of vessels during the ice floe and flood flows, as well as for the performance of activities related to vessel operations and its repair, renovation or construction.
- 4.2 **Public port users** are legal or natural persons who operate in a defined area of the public ports. During their activities they are obliged to abide by the instructions of the Transport Authority – Inland Waterway Transport Division (hereinafter referred to as “TA IWTD”), the Operator and the waterway administrator, as well as this Regulation.
- 4.3 **Vessel operator** is the owner of the vessel or the operator of the vessel, who operates the vessel on its own behalf and under a contract with the vessel owner. The vessel operator shall be obliged to pay the port charges and incur any penalties in accordance with the valid Schedule.
- 4.4 **Inland navigation** shall mean the operation of vessels on inland waterways and in ports, including those activities relating to such operations.
- 4.5 **A vessel** (hereinafter referred to as “ship”) is an inland waterway ship, a small vessel, a ferry, a floating machine or a floating establishment.
- 4.6 **Floating establishment** shall mean a vessel used on waterways and ports which is not intended for repeated relocation, such as a floating swimming pool, floating dock, landing pontoon, floating garage, botel, houseboat.
- 4.7 **A non-operated vessel** for the purposes of this port regulation shall mean:
  - a vessel without a valid ship certificate, which is also decommissioned for a period longer than one (1) month

- a vessel with a valid ship certificate which is decommissioned for a period of more than two (2) months following the decision of the vessel operator;
  - a vessel included in this category based on a decision of the Operator pursuant to Article 5, point 5.22.
- 4.8 Small vessel** shall mean a vessel with a hull length of up to 20 m intended for the transport of not more than 12 passengers and a vessel intended for sports and leisure purposes only, whatever its mode of propulsion, with a hull length between 2,5 m and 24 m, with the exception of vessels constructed or intended for pushing, towing or guiding in a side-by-side assembly of vessels, which are non-small vessels, ferries and floating machines.
- 4.9 Vessel's residence in the area of the public port** shall mean the period from the date of arrival of the vessel in the public port or the creation (production) of the vessel in the public port until the vessel's departure in accordance with the present Port regulation, provided that the deregistered vessel is not physically located in the area of the public ports. The day of arrival of the vessel in the public port shall also mean the day following the date of the vessel's deregistration by the original vessel operator, subject to the condition that the vessel remains in the public port.
- 4.10 Notification of arrival and departure** (hereinafter referred to as "NOAD") is a vessel's arrival/departure form which must be transmitted without delay to the relevant Operator by the vessel's master or other authorized person within 24 hours.
- 4.11 Schedule** shall mean a document approved by the Ministry of Transport and Construction of the Slovak Republic prepared in accordance with the provision of Article 5 par. 14 of the Inland Navigation Act and determines the method of collecting payments for the use of public ports on the waterways of the Slovak Republic, determines their amount and the calculation procedure.
- 4.12 Aquatorium** is the water area of a defined area of public ports.
- 4.13 Vessel master** shall be the appropriately qualified person responsible for compliance with the rules on board a vessel, assembly or floating body.
- 4.14 Captain office** - TA IWTD, Department of State Professional Supervision, which has, inter alia, the task of monitoring the movement of vessels on inland waterways in the Slovak Republic.
- 4.15 Office of VP, a.s. Bratislava** is located on the 1st floor of the TA IWTD building in Bratislava, Prístavná no. 10, room no. 212. Bratislava office is responsible for the administration of the Bratislava Public Port.
- 4.16 Office of VP, a.s. Komárno** is located on the ground floor of the TA IWTD building in Komárno, Ostrov Sv. Alžbety no. 3098, room no. 7. Komárno office is responsible for the administration of the public ports of Komárno and Štúrovo.
- 4.17 Lessee** is a natural person entrepreneur or legal entity which enters into a contractual relationship with the Company for the purpose of renting the Company's immovable property.
- 4.18 Lease Contract** (hereinafter referred to as "LC") is a bilateral written legal act concluded in accordance with the relevant generally binding legal regulations, the subject of which is the temporary provision of the Company's assets for the use by the Lessee.
- 4.19 Land** shall mean a property recorded in the Company's accounting records as an exclusive property of the Company.
- 4.20 Priority Investment Assets** (hereinafter referred to as "PIA") shall mean assets of the Company as defined by the Inland Navigation Act. The disposal of Priority Investment Assets is subject to a special regime and restrictions under the Inland Navigation Act. Priority Investment Assets may be leased by the Company

through a lease contract, with the lease period not exceeding 30 years. The validity of the lease is conditions by the approval of the Ministry of Transport and Construction of the Slovak Republic if the priority investment assets are leased for a period longer than one year or if the lease contract includes foresees a long-term repeated lease of longer than one year in favour of the lessee. The validity of the lease contract requires the approval of the Ministry of Transport and Construction of the Slovak Republic if it is repeatedly concluded with the same lessee for three consecutive years and the priority investment assets are leased for a maximum of one year or if the lease contract includes the right of repeated lease in total of a maximum of one year in favour of the lessee.

## Article 5

### Conditions for the use of public ports

- 5.1** Access to public ports is only permitted to users / visitors who will respect and comply with this Port regulation and restrictions related to port operations.
- 5.2** A vessel may enter the area of a public port only if it complies with the conditions specified by the applicable navigation measure of the Transport Authority and on condition that:
- the vessel will comply with the valid Port regulation following its entry into the area of the public port;
  - the vessel is in good technical and operational condition, has valid vessel's papers and is not prevented by any safety, hygiene, anti-epidemiological or fire regulations. The vessel operator shall be responsible for the vessel's proper condition and technical capacity. In particular, the vessel operator shall be responsible for ensuring that the vessel does not jeopardize the smoothness and safety of its operations, as well as the smoothness and safety of waterway traffic and the environment.
- 5.3** Before the entry of the vessel into the public port area, the vessel operator shall, through the vessel's master or other authorized person, without delay register the vessel on the frequency defined VHF radio channels in the Captain Office of the SPS - TA IWTD, according to the valid navigation measure issued by the Transport Authority, which stipulates the conditions of the Transport Authority to ensure the safety of navigation in public ports on the Danube. The vessel operator has the possibility to acquire information about the place of loading / unloading at the Dispatch centre of port services of Slovenská plavba a prístavy a.s. (hereinafter referred to as SPaP a.s.) - **channel no. 13**.
- 5.4** After the entry of the vessel into the port, the vessel operator shall, through the vessel's master or other authorized person, without delay and in accordance with this Port regulation, register the vessel in person with on-duty Operator's employee at the relevant branch or electronically at: **pobba@vpas.sk** (VP Office, as, Bratislava, for vessels not provided by the passenger port operator) or **pobkom@vpas.sk** (VP Office, a.s. Komárno, for vessels that are located in the port of Komárno and Štúrovo and which are not provided by the passenger port operator). Registration notice must include:
- Name and unique European Vessel Identification Number (ENI);
  - Vessel registration flag;
  - Last port (wherefrom the vessel arrived);
  - Vessel dimensions: Length (L) and beam (B) according to the entry of the data in the vessel's certificate;

- Vessel draft (not a condition);
  - Type and quantity of goods on board; in the case of dangerous goods loaded, notify the hazard class and the place of storage in the hold of the vessel;
  - Name and surname, or vessel operator's business name required to pay port charges, billing information, address, phone and email contact;
  - Activities to be carried out by the vessel in the port (unloading of goods, loading of goods, supply of potable water, replacement of crew, etc.);
  - For passenger ships, the number of passengers.
- 5.5** If a vessel changes its operator, who is obliged to pay the port charges for the stay in the port or activities performed in the port, the original operator shall be obliged to unregister the vessel from the port in writing within three (3) business days of such change and it shall be the obligation of the new operator to register the vessel in the port within three (3) business days of such a change. In the event of a breach of this obligation by the original or new operator, the Operator shall be entitled to invoice the port charges to the original vessel operator until the notification on the change of the vessel operator to the Operator or until the end of the vessel's stay in the public ports.
- 5.6** The vessel operator shall, through the vessel's master or other authorized person, without undue delay and no later than three (3) business days, report any change affecting the amount of the payment, the method of payment and the identification of the person who will be responsible for the payment of the port charges for the services related to the use of the port by the vessel (this also applies to vessels for which arrival and/or departure reports have not been submitted, vessels which have changed their name during the stay and/or have been re-registered under other flag as well as vessels that have been disposed of, partially disposed of or split during their stay). If the change is not notified within three (3) business days of the day on which it occurred, the Operator is not obliged to consider the fact which may have reduced the amount of the payment for use of the port by the vessel. In the event that the vessel operator enters into liquidation, commences restructuring, enters bankruptcy or execution proceedings against the vessel operator, the vessel operator shall notify the Operator of any such fact no later than three (3) business days from the occurrence of such fact and at the same time the Operator is entitled to request payment of a penalty according to the valid Schedule for any failure to notify payment such change. The payment of the penalty does not affect the Operator's right to increase the payment for the use of the port resulting from the unnotified change or the right for compensation for the damage caused, including damage exceeding the amount of the penalty.
- 5.7** In order to determine the quantities of loaded or unloaded tons of goods, the specification contained in the waybill or bill of lading which the user of the public port is obliged to present to the person authorized by the Operator upon arrival or departure from the area of the public port. The data entered in the ship's certificate are decisive for determining the size of the vessel. The vessel's master shall be obliged to produce a valid ship's certificate at the request of the Operator's employee.
- 5.8** Before the departure of the vessel from the area of the public ports, the vessel's master shall be obliged to unregister the vessel without delay with the Operator and pay the charges for the use of public ports in accordance with the valid Schedule. In case the owner or the operator of a vessel concluded a contract with the Operator on the payment of port charges, an invoice shall be issued and delivered for the use of the public port pursuant to the aforementioned contract and in accordance with the relevant legislation.
- 5.9** The operator of a floating establishment serving the landing of passenger ships shall submit a report no later

than on the fifth (5) day of the following month for all vessels that have landed on its floating establishment during the previous month. If the operator of a floating establishment operates more than one floating establishment, it shall assign each vessel with a name of the floating establishment on which the vessel has been moored. The payment of the port charges for passenger vessels shall be made by the operator of the floating establishment, unless otherwise agreed.

- 5.10** In case that the floating establishment operator wishes to make changes to the floating establishment, which may result in a change in the nature of the activity or the extension of activities, the operator of the floating establishment shall be obliged to request the Operator to issue an approval. In the event that the floating establishment operator makes changes to the floating establishment without the Operator's approval, the Operator shall impose a penalty on the floating establishment operator in accordance with the valid Schedule.
- 5.11** Berthing of stationary floating establishment, which is subject to registration, on a waterway or in a port is only allowed with a valid ship certificate and based on a decision to permit the berthing of a floating establishment on a waterway or in the port.
- 5.12** An application for the permission for berthing of a floating establishment in a port shall also include a Contract on a Future Contract with the Operator, subject of performance of which is in accordance with Article 6 par. 2 of the Inland Navigation Act. A Contract for a Future Contract with an Operator is not required if the request for permission for berthing of a floating establishment is made for a floating establishment owned by the Operator.
- 5.13** Pursuant to the Inland Navigation Act, the operator of a floating establishment located in the area of public ports of the SR is obliged to conclude with the Operator a Contract on a future contract subject of performance of which is in accordance with Article 6 par. 2 of the Inland Navigation Act. Upon signature of the Contract on a future contract, the floating establishment operator undertakes to:
- pay a deposit in accordance with the provisions of the Contract on a future contract,
  - pay a charge for the use of port position,
  - pay charges for the daily stay of the floating establishment in the area of public ports according to the valid Schedule,
  - after payment of the advance invoice for the first specified period of use of the port position (365 days), the operator of the floating establishment undertakes to invite the Operator to conclude the Contract on the use of public ports by the end of the first specified period. The invitation to conclude the Contract on the use of public ports shall be considered done with the sending of a request for the conclusion of the Contract on the use of public ports, with such request also including a copy of the decision of the Transport Authority to permit the berthing of the floating establishment at the port position. If the operator of the floating establishment does not invite the Operator to conclude the Contract on the use of public ports within the deadline and in manner outlined above, the Contract on a future contract shall be terminated and none of the parties shall be bound by its provisions.
- 5.14** In the event that a vessel has not departed from the area of the public port at a time notified under point 5.8 of this Article of the Port regulation, the vessel's master shall promptly submit a new departure notification, thereby automatically cancelling the original departure notification.
- 5.15** The vessel's master or a crew member authorized by the master, whose operator has not concluded a Contract on the payment of charges for port use with the Operator, is obliged to pay port charges and submit the relevant forms at the Operator's branch office from 07:00 to 19:00. Payment for services can also be made in advance.

- 5.16** The vessel's master or a crew member authorized by the master, whose operator has concluded a Contract on the payment of charges for port use with the Operator, may submit the appropriate forms at the Operator's branch office from 19:00 to 07:00 to the Operator's mailbox at the relevant branch.
- 5.17** In the case of a scheduled departure of the vessel between 19:00 and 07:00, the vessel's master or a crew member authorized by the master shall be obliged to pay the port charges for the use of the port in advance at the Operator's branch office from 07:00 to 19:00.
- 5.18** In the case a vessel was placed outside the public port aquatorium due repairs, the vessel operator or a person authorized by the operator shall be required to submit a relevant confirmation at the time of port charges payment. In case an invoice is issued, the vessel operator shall submit such confirmation within three (3) calendar days of the following month when the vessel was removed from the public port aquatorium. In the event of failure to comply with the deadline specified in the preceding sentence, the Operator shall not be obliged to consider the confirmation received after the deadline and shall be entitled to claim the payment of port charges for the vessel as if it were placed in a public port aquatorium.
- 5.19** If a vessel that has not been charged with port charges due to it being removed outside of the public port's aquatorium, is placed by the operator and/or the vessel's master back into the public port's aquatorium, the vessel's operator or master shall notify the Operator within three (3) calendar days.
- 5.20** Unless otherwise contractually agreed, the vessel operator shall be obliged to pay the port charges on a monthly basis, no later than on the fifth (5) day of the month following the month for which the port charges are paid. In the event of failure to comply with this obligation, the Operator shall issue to the vessel operator an invoice for the payment of port charges in accordance with applicable legislation. This does not affect the right of the Operator to apply sanctions against the vessel operator according to the valid Schedule.
- 5.21** In the event that the vessel operator fails to comply with the conditions specified in this Port regulation in the public port or the conditions of operation of the vessels laid down by the Transport Authority, the Operator may send him a request to remove or expel such vessel from the public port with the vessel operator bearing the costs. In the event that the vessel operator fails to comply with the obligations arising from the Operator's request and the safety of other port users is jeopardized by the vessel berthing in the port, the Operator may penalize the vessel operator under Art. 6 and take measures the costs of which will be invoiced to the vessel operator.
- 5.22** In the case of a long-term stay of a vessel or a floating establishment in a public port of more than 60 days, the Operator shall be entitled to include such a vessel or a floating establishment in the category of non-operated vessels for the purpose of levying port charges.
- 5.23** If the Operator places a vessel or a floating establishment in the category of non-operated vessels:
- 5.23.1** It shall do so on the basis of a notification by the owner or operator of the vessel or of the floating establishment about its decommissioning. A vessel or a floating establishment shall be placed in the category of non-operated vessels immediately upon receipt of a notification or request for decommissioning of the vessel or floating establishment, together with an annex which includes:
- a document proving the invalidity of the ship's certificate,
  - a SPS document on the incapacity of the vessel or the floating establishment for operation (e.g. decision to withhold the ship's certificate).
- 5.23.2** It shall do so on the condition that no activities shall be performed on the vessel or on the floating establishment which can only be carried out by operated vessels for payment.

- 5.23.3** The vessel operator will not be able to register the vessel for wintering during the winter period from 15 December to 15 March of the applicable calendar year.
- 5.24** The Operator shall transfer a vessel from the category of non-operated vessels to the category of operated vessels after receiving a written application from the owner or operator of the vessel. A mandatory annex to the application referred to in the previous sentence is a valid ship's certificate for the vessel concerned.
- 5.25** In case any activity is performed in the public port which is not related to port activity (such as video recording and others in the aquatorial and non-aquatorial area of the port), the applicant is obliged to request the Operator's consent to authorize such activity. Such activity may be charged according to the valid Schedule.
- 5.26** Unless otherwise stated in this Port regulation, any notices, reports, application and forms are to be submitted by the vessel operators to the on-duty employee of the Operator at the relevant Operator's Branch or electronically at the appropriate e-mail address: **pobba@vpas.sk** (VP Branch, a.s. Bratislava) or **pobkom@vpas.sk** (VP Branch, a.s. Komárno also for Štúrovo port).
- 5.27** Road traffic in public ports is governed by generally applicable road traffic legislation.
- 5.28** The Operator or a person authorized by the Operator shall be entitled to prevent access to roads located in public ports where vehicles are suspected:
- of having unsatisfactory technical condition to the extent which may endanger other users of the roads and/or
  - of being driven by a driver suspected of being under influence of alcohol or other addictive substances and/or
  - of being driven by a person who is unable to prove the origin of the cargo being transported, or a person without authorization to handle the cargo or if such person is not able to prove the purpose of the visit and/or
  - of being driven by a person who violates traffic rules on the roads until the opposite is proven and in order to verify (clarify) the validity of the suspicions in question the Operator or a person authorized by the Operator without undue delay calls upon the competent authorities who are authorized to verify the validity for such suspicions.
- 5.29** In case of persons who are not authorized to enter the public port area, these persons are required to apply for entry with the Company or with SPaP, a.s. Entry is only possible with the permission of the Company. This obligation does not apply in the case of persons who have a lease contract for the land or building in the area of public ports or in the case of a short-term visit to an entity located in public port area. The Lessee is liable for all persons having a legal relationship with the Company's Lessee.
- 5.30** For the purpose of checking whether the Lessee uses the Subject of Lease in the manner specified in the Contract, the Company has right of entry and access at any time to the Subject of Lease, unless otherwise specified in the Contract.
- 5.31** Lessee is obliged to:
- to use the Subject of Lease in accordance with the purpose of the Lease, generally binding legal regulations of the Slovak Republic, Slovak technical standards, sanitary, safety and fire-fighting generally binding legal regulations currently valid and effective in the territory of the Slovak Republic so as not to cause any damage of property, life or health of persons, while the Lessee is prohibited from using any substances, procedures and equipment causing harm to the environment by vapour, noise and vibration or otherwise, above the limits permitted by applicable standards in the territory of the Slovak Republic

- use the Subject of Lease with due diligence;
- use the Subject of Lease in a manner that is not, within the meaning of the provision of Article 127 of the Civil Code, excessively restricting or preventing the use of surrounding property by other Lessees, owners or other authorized persons;
- ensure that its activities do not pollute the Subject of Lease or its surroundings, the area of public ports according to the Inland Navigation Act, ground waters and surface waters by petroleum or other harmful substances;
- maintain the Subject of Lease throughout the term of the Contract in condition as it was taken over from the Company, taking into account normal wear and tear;
- pay the Lease in the manner and time agreed in the Contract;
- at its own expense, ensure maintenance of the Subject of the Lease.

**5.32** The Lessee:

- is not entitled to sublease the Subject of the Lease or any part thereof without prior written consent of the Company;
- is authorized to carry out construction modifications, any technical improvements or other changes to the Subject of the Lease only upon prior written consent of the Company, at its own expense, unless the Parties agree otherwise, subject to applicable generally binding legal regulations. If the Lessee makes any construction modification, technical improvements or other changes to the Subject of the Lease without the prior consent of the Company, it shall not be entitled to any reimbursement of the costs thus incurred or the consideration for what may have been considered increased value of the Subject of the Lease. In such a case, the Lessee is obliged to return Subject of the Lease back into the original condition at its own expense;

**5.33** Any Lessee of the public port area or a person (applicant for entry to the port area) is obliged to comply with the following regulations:

Reviews:

- The Parties agree that review reports and professional inspections of buildings and equipment related to the Subject of the Lease owned by the Company shall be provided by the Lessee at its own expense in accordance with the applicable regulations;
- Review reports of any equipment owned by the Lessee shall be provided by the Lessee at its own expense in accordance with the applicable regulations, and the Lessee shall also bear full responsibility for the technical condition of this equipment.

Environment:

- The Lessee is committed to complying with environmental protection legislation. In the event of violation of legislation, the Lessee shall bear the costs of full potential damage to the environment, health and property as well as any penalties levied by the state and other bodies;
- The Lessee is obliged to clear the land after the lease and to clean the land in case of any ecological pollution in connection with its use;
- Disposal of all produced waste will be ensured by the Lessee at its own expense with the Lessee being also obliged to comply with the Waste Act no. 79/2015 as amended.

Occupational health and safety:

- The Lessee undertakes to fully comply with the provisions of Act no. 124/2006 Coll. on Occupational

Health and Safety and on Amendments and Supplements to Certain Acts, as amended, and all applicable legal standards concerning occupational health and safety / Government Regulation, Decrees, Slovak Technical Standards;

- In case of a one-time entry to the port, the person (applicant or organizer) is obliged to provide the following for all persons who have been granted access to the port: a reflective vest and a protective helmet shall be worn in the public port areas, they shall comply with regulations in relation to OHS and fire protection, they shall comply with the Public Health Act, they shall comply with the Port regulation of public ports of the SR, they shall not enter pontoons and other technical facilities in the port, they shall not restrict the operation and activities of third parties in the port. The person who was granted entry hereby acknowledges that he/she enters the public port area at his/her own risk and is liable for any damage that may be incurred by the Company or third parties to health or property in connection with his/her entry and activity in the public port area, including for damages caused by third parties. The right of the Company to claim damages is not affected.
- In accordance with Article 21 par. 2 of the Act no. 124/2006 Coll. the Lessee undertakes, at his own expense, to ensure the implementation of preventive and protective services, which include the Security Technical Service (Article 22) and Occupational Health Service (Article 26);
- In the event of a breach of legislation, the Lessee shall bear in full any potential damage to the environment, health and property, as well as sanctions levied by the state and other bodies.

Fire protection:

- In order to ensure the protection of life and health of employees in leased premises, protection of property and the environment against fire and in accordance with Article 6 par. 2 of Act no. 314/2001 Coll. on fire protection, as amended, the Lessee undertakes to fully comply with the provisions of this Act and all applicable legal regulations relating to fire protection;
- In accordance with Article 4 point o) of Act no. 314/2001 Coll. on Fire Protection, as amended, the Lessee undertakes to ensure, at its own expense, the fulfilment of the stipulated duties and specified tasks in the field of fire protection by persons having the required professional competence or special authorization;
- In the event of a breach of regulations, the Lessee shall bear the costs of all potential damages to health and property, as well as sanctions levied by the state and other bodies.

## Article 6

### Penalties

- 6.1** The Operator is entitled to require the user of public ports (including the vessel operator) to impose a fine according to the valid Schedule for any breach of obligations stipulated in the Port regulation.
- 6.2** The user of public ports shall not be exempted from the obligation to remedy the situation for which the fine was imposed by paying the fine under point 6.1. Payment of the penalty under 6.1 does not affect the claim for damages, including damages exceeding the penalty.
- 6.3** The user of the public ports is obliged to compensate any damages caused by its breach of the obligation stipulated in the Port Regulations and generally binding legal regulations.
- 6.4** If the user of the public ports repeatedly violates the obligation for which it was already fined, the Operator is

entitled to re-impose a penalty on the public port user for such a violation.

- 6.5** The user of public ports is obliged to pay the Operator the penalty according to the valid Schedule, even if the breach of obligations under the Port regulation was caused by negligence. The Operator may waive the penalty under this Article imposed on the user of public ports, upon request and in case of justified causes.
- 6.6** In case the vessel operator fails to comply with the payment discipline or fails to pay the port charge, the Operator shall send a request to the vessel operator to pay the liability without delay. In the event that the vessel operator fails to fulfil the obligation under the sent notice, it is obliged to pay the Operator interest on late payment in the amount of 0.05% of the amount due for each day of delay.
- 6.7** In the event of late payment or non-payment of a port charge, the Operator shall be entitled to inform all port operators on the Danube and the navigation public of the late payment or non-payment of the charge by the specific users of the public ports or to disclose the name of that user of the public ports on its website until payment of the charge by the relevant user.
- 6.8** The provisions of this Article are without prejudice to the Operator's right to damages.
- 6.9** The Lessee shall be obliged to compensate any damages caused by the Lessee or by persons who use the Subject of the lease, by breaching the obligations resulting from the Contract and generally binding legal regulations effective in the territory of the Slovak Republic. The Lessee shall be liable for any damage caused by persons to whom the Lessee provided access to the Subject of lease. The Lessee shall not be liable for any damage caused to the Subject of Lease by persons acting with the consent or at the direction of the Company.
- 6.10** In addition to the contractual penalty, the Company is entitled to full compensation of damages, including damages exceeding the amount of the contractual penalty, if the Lessee breaches an obligation that is secured by a contractual penalty and which results in damages to the Company.
- 6.11** The Company is obliged to compensate any damages caused to the Lessee by breach of obligations arising from the Contract or generally binding legal regulations.
- 6.12** Should damage arise as a result of a breach of rights and obligations under the Contract by one of the Parties, the Parties undertake to prepare a record on the issue without undue delay. The record shall include a description of the event and breach of the contractual obligation by the Party which is causally related to the occurrence of the damage and a statement by the Party which caused the damage. The record shall also include the calculation of the damage incurred if its amount can be calculated without undue delay. The Parties also undertake to include in the minutes the method of compensation. In case the amount of the damage cannot be quantified in the record, the amount of the damage and the method of its compensation can be agreed in a separate agreement on damages. If no agreement is reached on the settlement of the damages, the Party that has incurred the damage is entitled to claim the damages pursuant to the applicable legal regulations of the Slovak Republic.
- 6.13** The amount of the contractual penalty for the individual breach of the Lessee's obligation is specified in the table Breach of contractual obligations by the Lessee, which forms a part of the Lease Contract.

## Article 7

### Protection of vessels and areas of public ports of the SR

- 7.1 If the TA IWTD issues a Navigation measure determining the specific conditions for safe operation of vessels in public ports, all users of public ports are obliged to comply with such Navigation measure.
- 7.2 In the case of issuance of a Navigation measure on wintering of vessels in the Slovak Republic:
- 7.2.1 vessel operators applying for wintering positions are obliged to report the anticipated type and number of vessels before wintering;
- 7.2.2 port users, vessel masters and vessel operators are obliged to follow the guidelines issued by the Operator before and during wintering of vessels in the port. The guideline will specify the positions based on the following key for:
- vessels not carrying dangerous goods,
  - vessels carrying dangerous goods,
  - other vessels;
- 7.2.3 vessel masters shall comply in full under the regulations resulting from the Navigation measure on wintering of vessels.
- 7.2.4 the Operator is the only person authorized to invoice the charge for wintering of vessels (in the period from 15 December to 15 March) in the location of public ports of Bratislava and Komárno.
- 7.3 Monitoring of vessels classified in the registration of non-operating vessels in public ports shall be provided by the vessel operator at its own expense. By ensuring that the vessel is monitored, its operator shall not be relieved of liability for any damage caused to the Operator, other users of public ports or third parties in connection with the presence of the vessel in the public port. The Operator reserves the right to perform acts and measures in the area of public ports in order to minimize the risk of potential damage. The costs associated with these operations shall be fully borne by the vessel operator
- 7.4 The user of public ports, who contractually uses the port location situated in the defined area of public ports of the SR, is obliged to maintain its port position in desired condition at its own expense.
- 7.5 It is possible to dispose of the municipal waste in the area of public ports only in places exclusively designated for this purpose.
- 7.6 In the area of the public ports it is prohibited, in particular to:
- throw away, pour out and let any objects or substances fall or leak, if these can create a navigational obstacle or danger to other waterway users and pollute the watercourse,
  - throw away, pour out or let other substances which may cause deterioration of surface water to flow into the aquatorial part of the public port,
  - leave any municipal waste at locations other than those specifically designated and properly labelled,
  - carry out any repair and other work on the ship's body behind the deck.
- 7.7 In case of leakage of objects or substances mentioned in paragraph 7.6. of this Port regulations or in the case of risk of such leakage, the vessel's master shall inform the Operator and the Slovak Environmental Inspectorate or the nearest competent authority (Fire and Rescue Corps) without any delay, specifying the nature and location of the leak.
- 7.8 For the disposal and handling of municipal waste from the foreign vessels located in the port, containers are placed in Bratislava at the positions MFD 9 and MFD 53 and in Komárno at position MFD 25. The disposal of municipal waste is handled by the on-duty staff of the Operator.
- 7.9 Organisation of waste disposal:
- Bratislava port:**

- **location no. MFD 9** - r.km 1867.290 – possibility to dispose of municipal waste,
- **location no. MFD 53** - r.km 1865.880 – on the pontoon SPaP-P-65, is the possibility of disposal of drainage water on business days from 7:00 to 15:00. When disposing of waste, it is mandatory to report it via e-mail to bunker@spap.sk.

**Komárno port:**

- **location no. MFD 25** - r.km 1767.470. possibility to dispose of municipal waste.

- 7.10** In the event of accumulation of a greater number of vessels due to cessation or restriction of traffic and wintering, the Operator shall, at the request of the vessel masters, provide bulk containers for payment. For liquid waste disposal purposes, the vessel's master may contact the Operator's servicing employee who will arrange for such disposal on the basis of a confirmed order by the vessel's master or vessel's operator.
- 7.11** Drinking water supplies to vessels are carried out at the Bratislava port at MFD-9 pontoon P-12 (r. km 1867.290), according to the Operator's instructions and in accordance with the Schedule. The vessel operator may be provided information by the SPaP, a.s. port services dispatch on the possibility of connecting the vessel to a power supply in the cargo port of Bratislava and the cargo port of Komárno. The connection of the vessel to the power supply is charged according to the tariff of the electricity provider – company SPaP a.s.
- 7.12** Any extraordinary event shall be reported immediately by the public port user or by a person authorized by the user without delay by radio to the SPS Bratislava and Štúrovo channel 22, SPS Komárno channel 18. to the Operator by telephone at the relevant branch or in person. This is without prejudice to the obligation to report such an event to the rescue service and other relevant authorities.

**Important phone numbers:**

Verejné prístavy, a. s., Bratislava office, Phone no.: +421 2/ 2062 0533, +421 910 309 099

Verejné prístavy, a. s., Komárno office, Phone no.: +421 2/ 2062 0534, +421 35/ 3260 105, +421 911 309 095

SPS, Captain office Bratislava Phone no.: +421 2/ 4877 7626, +421 917 562 069

SPS, Captain office Komárno Phone no.: +421 35/ 6921 853, +421 917 562 079

SPS, Captain office Štúrovo Phone no.: +421 36/ 6350 308, +421 917 562 068

Fire service Phone no.: 150, 112

Rescue Service Phone no.: 155, 112

Police Phone no.: 158, 112

# PORT REGULATION – PART TWO

## Article 8

### Bratislava port

**8.1** The port of Bratislava lies on both banks of the Danube river from r. km 1 871.350 to r. km 1 862.000. It consists of 4 port pools on the left bank, where cargo handling is taking place and at the same time these serve as a protective part of the port in case of unfavourable navigation conditions (high water, ice floe, etc.). In terms of activities, the Bratislava port is divided into thirteen sections, i.e. sections 0 to 12. The Operator establishes a buffer zone under the bridges which intersect the Danube in the port aquatorium. This area is 20 meters downstream and upstream for safety reasons. It is forbidden to tie any FE in this area.

Bratislava Port														
Port section:	Passenger port	MFD	Winter port				MFD	Pálenisko pool				Lodenica pool		MFD
Section:	0.	1.	2.	3.	4.	5.	6.	7.	8.	9.	10.	11.	12.	
Number of port positions:	31	12	14	13	4	5	9	8	3	3	8	5	8	
Position types:	Parking position	Handling and parking positions						Handling positions		Parking and repair positions		Parking positions		

**8.2** This section of the Port regulation sets out the handling, service, repair, parking and other non-specified positions, conditions and method of anchorage, mooring and admission of the vessel into the port. The method of making fast is recommended and the making fast described at each position is minimal. The location and mooring of a given floating establishment is determined by the decision on the approval of parking issued by the SPS. The conditions and method of anchorage of vessels are set out in the currently valid navigation measure issued by the Transport Authority, including an Annex setting out the SPS conditions to ensure the safety of navigation in public ports.

**8.2.1 SECTION 0. - r.km 1870.450 to 1867.400** is located on both banks of the main flow of river Danube. All floating establishments located in this section are required to have a valid berthing approval decision issued by the SPS. Positions in **r. km 1870.450 to 1867.400** are primarily intended to accommodate floating establishment, or pontoon, which is used for landing of passenger cruise or personal cabin vessels and with the consent of the Operator of the public ports of the SR it is possible to place a floating establishment on which services are provided or activities are performed that do not conflict

with the Port regulation or other generally binding legal regulation. For the public port users, the Operator provides information on the approximate navigation depth with the description of port positions at water level Bratislava 300 cm.

This section consists of the following positions:

**Position PPBA 1 - left bank r. km 1 870.450 to 1 870.330** it is primarily intended to make fast the floating establishment on which services are provided or activities are performed which are not in conflict with the Port regulations or other generally binding legislation

**Position PPBA 2 - left bank r. km 1 870.250 to 1 870.200** it is primarily intended to make fast the floating establishment on which services are provided or activities are performed which are not in conflict with the Port regulations or other generally binding legislation. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.0 m.

**Position PPBA 3 - left bank r.km 1870.180 to 1870.090** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.5 m.

**Position PPBA 4 - left bank r.km 1870.079 to 1870.026** it is primarily intended to make fast the floating establishment on which services are provided or activities are performed which are not in conflict with the Port regulations or other generally binding legislation. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.5 m.

**Position PPBA 5 - left bank r.km 1870.000 to 1869.860** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.

**Position PPBA 6 - left bank, r.km 1869.840 to 1869.720** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.



Source: VP, a.s.

**Position PPBA 7 - left bank, r.km 1869.700 to 1869.640** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.5 m.

**Position PPBA 8 - left bank, r.km 1869.620 to 1869.500** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.0 m.

**Position PPBA 9 - left bank, r.km 1869.480 to 1869.400** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.0 m.

**Position PPBA 10 - left bank, r.km 1869.370 to 1869.240** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.0 m.

**Position PPBA 11 - left bank, r.km 1869.230 to 1869.150** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.0 m.



Source: VP, a.s.

**Position PPBA 12 - left bank, r.km 1869.050 to 1868.995** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.0 m.

**Position PPBA 13 - left bank, r.km 1868.976 to 1868.896** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.0 m.

**Position PPBA 14 - left bank, r.km 1868.890 to 1868.840** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.

**Position PPBA 15 - left bank, r.km 1868.830 to 1868.780** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.

**Position PPBA 16 - left bank, r.km 1868.760 to 1868.700** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.

**Position PPBA 17 - left bank, r.km 1868.680 to 1868.600** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.



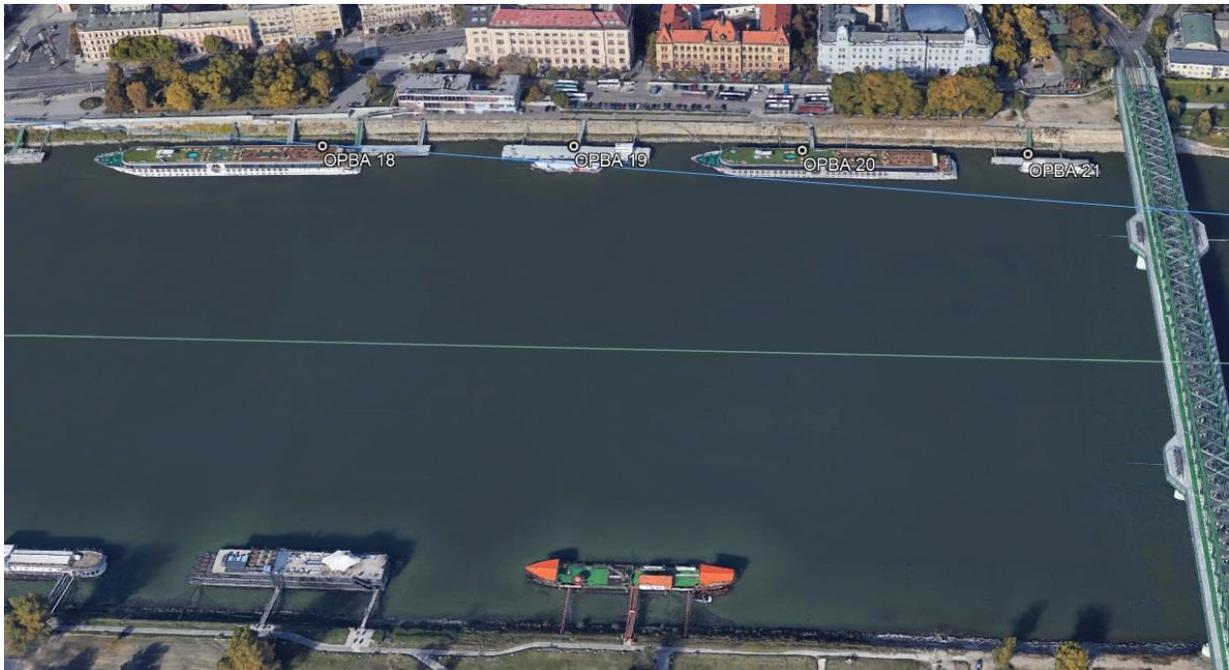
Source: VP, a.s.

**Position PPBA 18 - left bank, r.km 1868.580 to 1868.470** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.5 m.

**Position PPBA 19 - left bank, r.km 1868.460 to 1868.360** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.5 m.

**Position PPBA 20 - left bank, r.km 1868.350 to 1868.250** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.5 m.

**Position PPBA 21 - left bank, r.km 1868.240 to 1868.140** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.



Source: VP, a.s.

**Position PPBA 22 – left bank, r.km 1868.050 to 1867.650** it is intended for floating establishments (pontoons) which will primarily serve for the landing of vessels. If necessary, the Operator reserves the right to reallocate that port location to parts bordered by river kilometres. The location of floating establishment shall be in accordance with the SPS berthing approval. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.

**Position PPBA 23 - left bank, r.km 1867.620 to 1867.500** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.

**Position PPBA 24 - left bank, r.km 1867.490 to 1867.400** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.0 m.



Source: VP, a.s.

**Position PPBA 25 - right bank, r.km 1868.900 to 1868.800** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 4.0 m.

**Position PPBA 26 - right bank, r.km 1868.790 to 1868.720** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 4.5 m.

**Position PPBA 27 - right bank, r.km 1868.710 to 1868.610** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.5 m.

**Position PPBA 28 - right bank, r.km 1868.600 to 1868.510** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.5 m.

**Position PPBA 29 - right bank, r.km 1868.500 to 1868.410** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 3.5 m.

**Position PPBA 30 - right bank, r.km 1868.400 to 1868.290** is primarily intended for the location of a floating establishment, or pontoon, which serves for landing of passenger cruise or personal cabin vessels. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.5 m.

**Position PPBA 31- right bank, r.km 1868.280 to 1868.200** it is primarily intended for making fast of the floating establishment, or pontoon where services are provided, or activities are performed which are not in conflict with the

Port regulation or other binding legal regulation. With water level of 300 cm (water meter station Bratislava) the navigation depth is approximately 2.5 m.



Source: VP, a.s.

**8.2.2 SECTION 1. - r.km 1867.290 to 1866.250** is located on the left bank of the main flow of the river Danube. All floating establishments located in this section must have a valid berthing approval decision issued by the SPS. This section consists of the following positions:

**Position MFD 9 - left bank, r.km 1867.290 to 1867.210** is intended for floating establishment, which is used to make fast max. of 3 vessels made fast side by side with a max. beam of 35 m, with the first and third vessel being anchored. Freight ships pushed convoys 1 + 1 and 1 + 2 are obliged to anchor at the water level Bratislava 400 cm and higher. A pushed convoy in 1+3 or 1+2+2 arrangement may be parked at the pontoon only on its own and if two vessels are loaded in the assembly, they will be anchored. Position serves to inspect vessels by the SPS. The Operator provides the vessels with the possibility to supply drinking water and hand over municipal waste produced in the Slovak Republic.

**Position MFD 10 - left bank, r.km 1867.210 to 1867.130** is intended for floating establishment, which is used to make fast max. of 3 vessels made fast side by side with a max. beam of 35 m, with the first and third vessel being anchored. Freight ships pushed convoys 1 + 1 and 1 + 2 are obliged to anchor at the water level Bratislava 400 cm and higher. A pushed convoy in 1+3 or 1+2+2 arrangement may be parked at the pontoon only on its own and if two vessels are loaded in the assembly, they will be anchored. In the case of an emergency on board the vessel (quarantine), this position serves to make fast the affected vessel.

**Position MFD 11 - left bank, r.km 1867.130 to 1867.050** is a service position for vessels not carrying dangerous goods on which two vessels may be located with a beam of up to 24 m, where the bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 12 - left bank, r.km 1867.050 to 1866.970** is a service position for vessels not carrying dangerous goods on which two vessels may be located with a beam of up to 24 m, where the bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 13 - left bank, r.km 1866.970 to 1866.890** is a service position for vessels not carrying dangerous goods on which two vessels may be located with a beam of up to 24 m, where the bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 14 - left bank, r.km 1866.890 to 1866.810** is a service position for vessels not carrying dangerous goods on which two vessels may be located with a beam of up to 24 m, where the bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 15 - left bank, r.km 1866.810 to 1866.730** is a service position for vessels not carrying dangerous goods on which two vessels may be located with a beam of up to 24 m, where the bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 16 - left bank, r.km 1866.730 to 1866.660** is a service position for vessels not carrying dangerous goods on which two vessels may be located with a beam of up to 24 m, where the bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 17 - left bank, r.km 1866.660 to 1866.590** is a parking position for two vessels with a beam of up to 24 m, which do not carry any dangerous goods. The bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 18- left bank, r.km 1866.590 to 1866.510** is a parking position for two vessels with a beam of up to 24 m, which do not carry any dangerous goods. The bank vessel will be anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.

**Position MFD 19A - left bank, r.km 1866.510 to 1866.450** is a handling position for vessels not carrying any dangerous goods on which it is prohibited to park vessels so as to interfere with the bridge protection zone and where only one vessel may be made fast and anchored. If necessary, it will serve as a parking position for vessels in quarantine condition after initial activities performed on MFD 9 location.

**Position MFD 19B (TRALCIT)- left bank, r.km 1866.350 to 1866.250** is a parking position for vessels not carrying any dangerous goods on which it is prohibited to park vessels so as to interfere with the bridge protection zone and the max. permissible beam is 24 m, with the bank vessel being anchored, made fast by two bank ropes, a cross rope and a rear rope. If the second vessel is loaded, it must be anchored.



Source: VP, a.s.

**8.2.3 SECTION 2.- r.km 1866.250 – Winter port** – forms the southern bank of the southern pool with 750 m in length, leading to the front bank (boat lift) and continues 400 m to the north bank of the south pool.

- **On the west (front) bank** there is a boat lift, designed for the maintenance, repair and reconstruction of vessels.
- **South bank** is 750 m in length and has nine parking positions (used by SPaP, a.s.) and one refuelling position for vessels.

**Position P-1** – parking position, which is intended for vessels with their own propulsion on pontoons up to max. beam of 35 m.

**Position P-2** - parking position, which is intended for vessels with their own propulsion on pontoons up to max. beam of 30 m.

**Position P-3** - parking position, which is intended for vessels with their own propulsion on pontoons up to max. beam of 30 m.

**Position P-4** - parking position, which is intended for vessels with their own propulsion on pontoons up to max. beam of 30 m.

**Position P-5** - parking position, which is intended for vessels with their own propulsion on pontoons up to max. beam of 30 m.

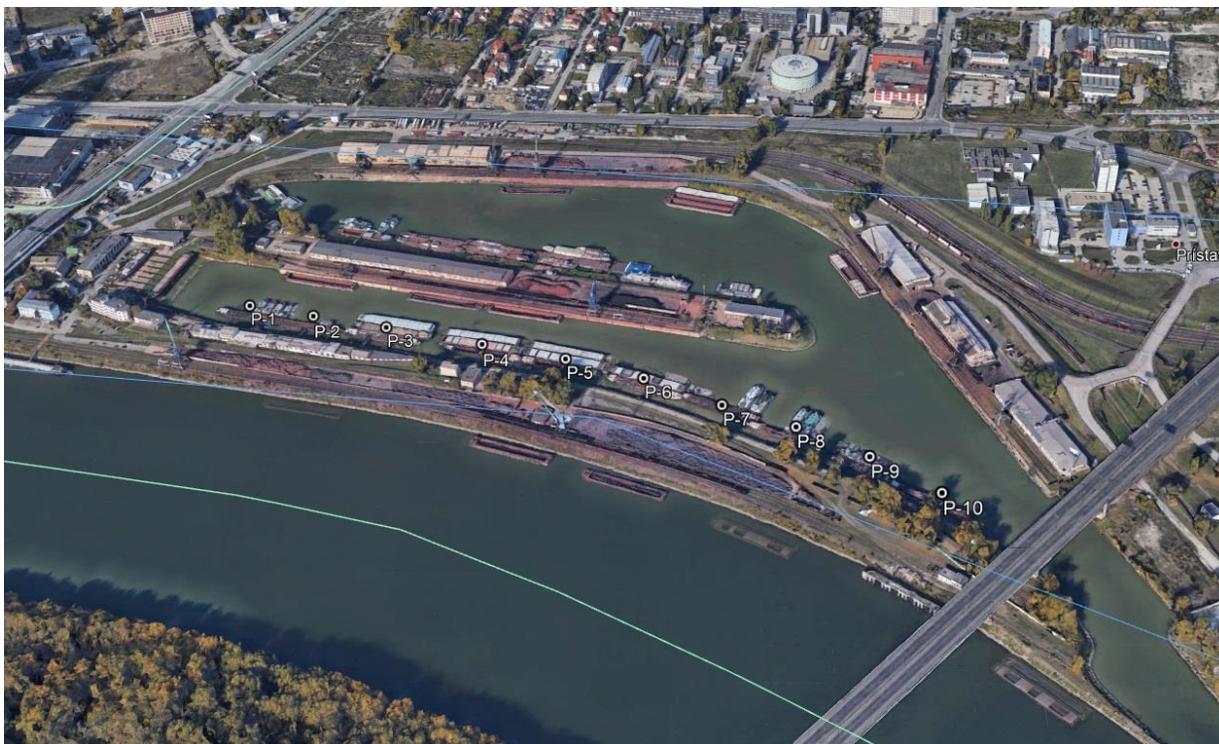
**Position P-6** – parking position which is intended for vessels without their own propulsion on pontoons, not carrying any dangerous goods, up to max. beam of 24 m.

**Position P-7** - parking position which is intended for vessels without their own propulsion on pontoons, not carrying any dangerous goods, up to max. beam of 24 m.

**Position P-8** - parking position which is intended for vessels without their own propulsion on pontoons, not carrying any dangerous goods, up to max. beam of 24 m.

**Position P-9** - service position where vessels up to max. beam of 26 m may be parked.

**Position P-10** - refuelling position, where one refuelled vessel can be parked



Source: VP, a.s.

- **North bank** is 400 m in length and has one parking and four handling positions. This section consists of the following positions:

**Position NB 20** - a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24 m

**Position NB 21**- a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24 m.

**Position NB 22**- a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24 m.

**Position NB 23**- a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24 m.



Source: VP, a.s.

**8.2.4 SECTION 3. - r.km 1866.250- Winter port** – forms the southern bank of the north pool with 590 m in length, leading to the front bank and continues 660 m in length on the north shore of the north pool

- **South bank** is 590 m in length and has eight parking positions from the front to the mouth of the entrance part. This section consists of the following positions:

**Position SB 1** – parking position intended for vessels of the watercourse administration company, i.e. Slovenský vodohospodársky podnik, š.p. OZ Bratislava, Danube office with 40 m in length from the front part of the pool.

**Position SB 2** – parking position intended for berthing of SPS vessels and port Operator vessels in length of 80 m.

**Position SB 3** - parking position intended for vessels of the watercourse administration company, i.e. Slovenský vodohospodársky podnik, š.p. OZ Bratislava, Danube office with 80 m in length.

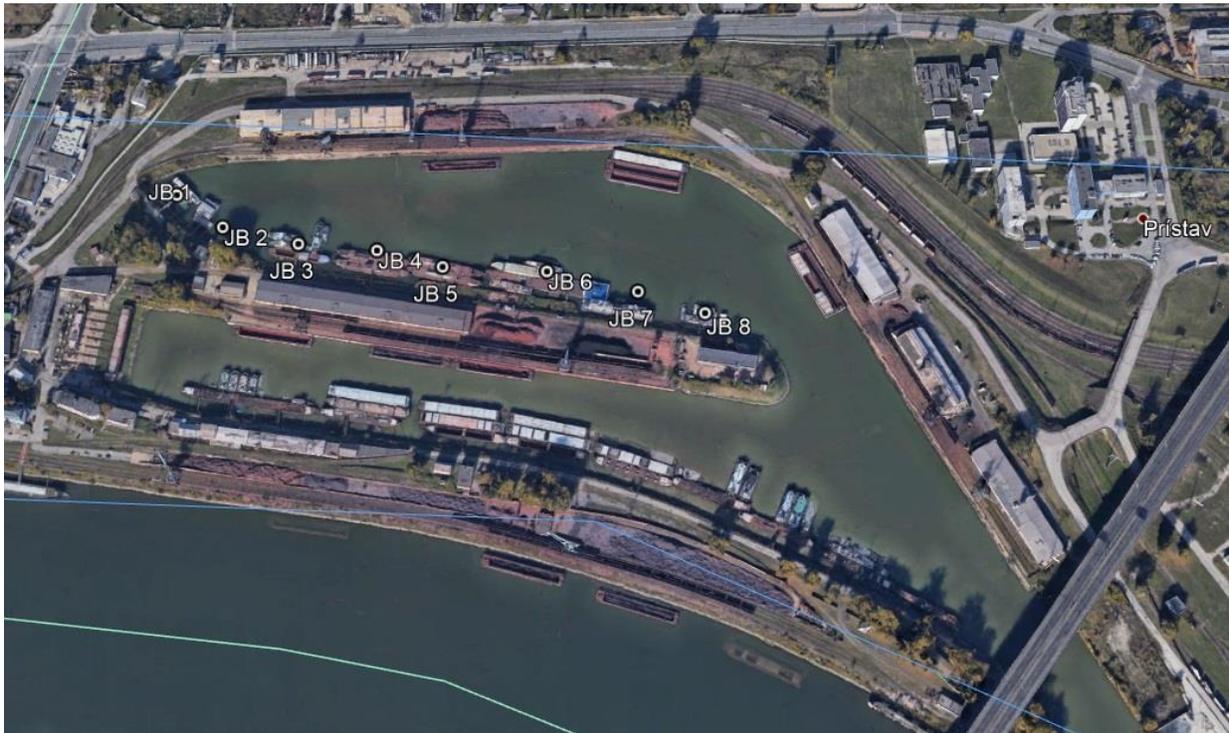
**Position SB 4** – parking position for pontoons and passenger vessels up to max. beam of 44 m in length of 80 m.

**Position SB 5** - parking position for pontoons and passenger vessels up to max. beam of 44 m in length of 80 m.

**Position SB 6** - parking position for maintenance, repair and reconstruction of vessels up to max. beam of 34 m in length of 90 m.

**Position SB 7** – parking position for vessels waiting for repairs up to max. beam of 44 m and in length of 90 m, berthing time is limited to 1 month, after this period berthing is only allowed with the consent of the port Operator.

**Position SB 8** - parking position intended for vessels, pontoons, floating garages of the Ministry of Interior of the Slovak Republic in the length of 50 m



Source: VP, a.s.

- **north bank** has two parking positions and three handling positions in direction from the front bank to the beginning of the 4th section in length of 660 m.

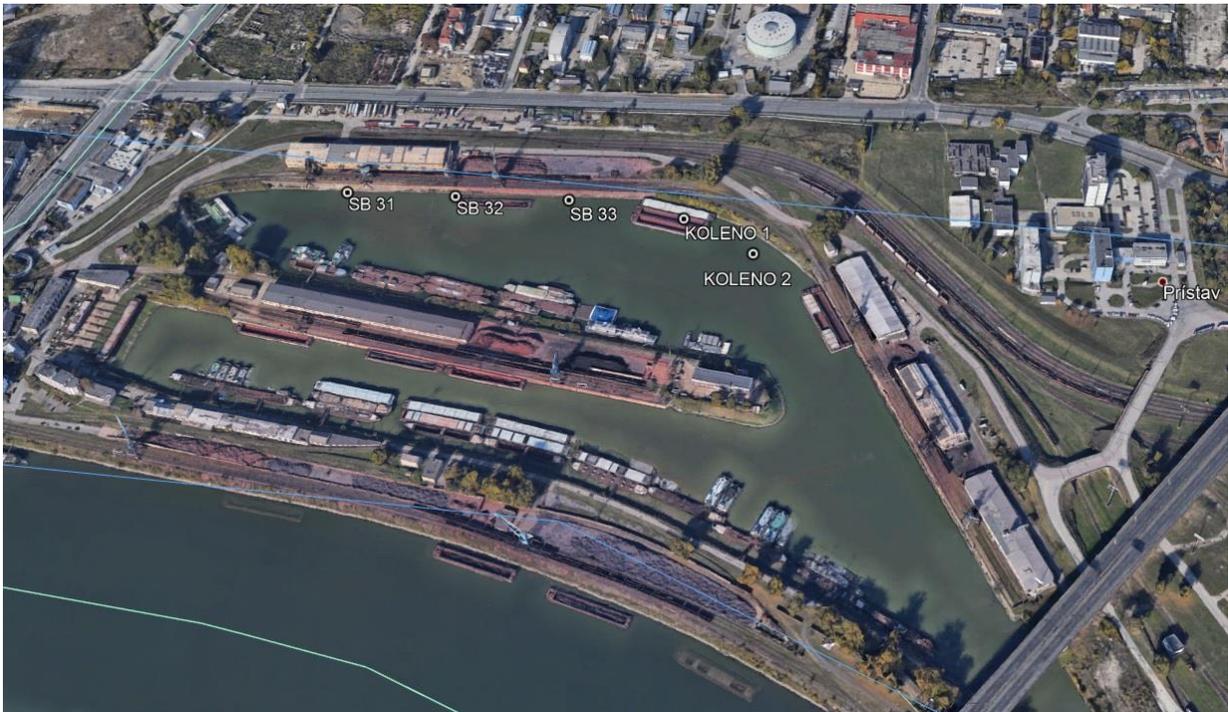
**Position NB 31** - a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24m.

**Position NB 32** - a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24 m.

**Position NB 33** - a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24m.

**Position NB BEND 1**- parking position intended for the berthing of vessels not carrying dangerous goods with a beam up to 35 m

**Position NB BEND 2**- parking position intended for the berthing of vessels not carrying dangerous goods with a beam up to 35 m.



Source: VP, a.s.

**8.2.5 SECTION 4.- r.km 1866.250- Winter port** – forms the northern bank of the northern pool from the parking position BEND 2 towards the mouth of the entrance part with a length of 400 m. It has one parking and three handling positions as follows:

**Position NB 41-** parking position where vessels not carrying dangerous goods may be parked with a beam up to 24m.

**Position NB 42** - a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24m.

**Position NB 43** - a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24m.

**Position NB 44** - a handling position where vessels not carrying dangerous goods may be parked with a beam up to 24m.



Source: VP, a.s.

**8.2.6 SECTION 5.- r.km 1866.000 - 1865.400** – located on the left bank of the main flow of river Danube. It consists of five handling and service positions on floating establishments (pontoons) of mineral oil tranship point - MOT. This section consists of the following positions:

**Position MFD 51 - left bank, r.km 1866.000 to 1865.890** - handling position where one vessel is made fast to the floating establishment (pontoon) and is anchored and made fast by one bank rope.

**Position MFD 52 - left bank, r.km 1865.890 to 1865.790** - handling position where one vessel is made fast to the floating establishment (pontoon) and is anchored and made fast by one bank rope.

**Position MFD 53 - left bank, r.km 1865.790 to 1865.650** - handling position, where refuelling of small and large vessels with own propulsion, collection of drainage, sewage and municipal waste is possible on floating establishment (pontoon).

**Position MFD 54 - left bank, r.km 1865.650 to 1865.580** - handling position, where refuelling of small and large vessels with own propulsion, collection of drainage, sewage and municipal waste is possible on floating establishment (pontoon).

**Position MFD 55 - left bank, r.km 1865.580 to 1865.400** - handling position where one vessel carrying dangerous goods is made fast to the floating establishment (pontoon) and is anchored and made fast by one bank rope.



Source: VP, a.s.

**8.2.7 SECTION 6.- r.km 1865.400 - Pálenisko** – forms the west bank of the Pálenisko pool from the front bank to the mouth of the entrance section to the pipeline with a length of 800 m. It has nine parking and handling positions, with three parking positions exclusively intended for making fast of vessels carrying dangerous goods. This section consists of the following positions:

**Position WB 61-** handling position intended for vessels not carrying dangerous goods in the length of 85 m, up to max. beam of 24 m.

**Position WB 62-** handling position intended for vessels not carrying dangerous goods in the length of 85 m, up to max. beam of 24 m.

**Position WB 63-** handling position intended for vessels not carrying dangerous goods in the length of 85 m, up to max. beam of 24 m.

**Position WB 64-** handling position intended for vessels not carrying dangerous goods in the length of 85 m, up to max. beam of 24 m.

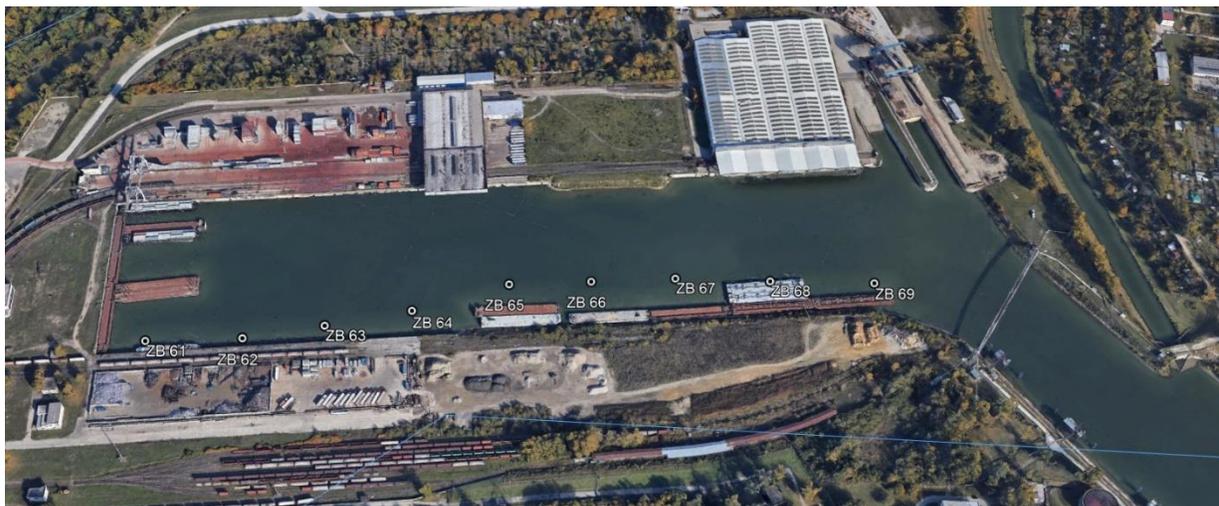
**Position WB 65** - parking position intended for berthing of vessels not carrying dangerous goods in the length of 85 m, up to max. beam of 80 m.

**Position WB 66** - parking position intended for berthing of vessels not carrying dangerous goods in the length of 85 m, up to max. beam of 80 m.

**Position WB 67** - parking position intended for berthing of vessels carrying dangerous goods in the length of 80 m, up to max. beam of 80 m.

**Position WB 68** - parking position intended for berthing of vessels carrying dangerous goods in the length of 100 m, up to max. beam of 80 m.

**Position WB 69** – parking position intended for berthing of vessels carrying dangerous goods up to max. beam of 35 m.



Source: VP, a.s.

**8.2.8 SECTION 7.- r.km 1865.400 -Pálenisko** – forms the northern (front) bank and the eastern bank of the Pálenisko pool. It has eight handling and parking positions as follows:

**Position FB 70-** parking position intended for perpendicular making fast of vessels with and without their own propulsion with max. length of 110 m, which must not interfere with the vessels in the handling positions of the east and west banks.

**Position EB 71** – trimodal handling position intended for vessels not carrying dangerous goods in length of 100 m, up to max. beam of 24 m

**Position EB 72** - trimodal handling position intended for vessels not carrying dangerous goods in length of 100 m, up to max. beam of 24 m.

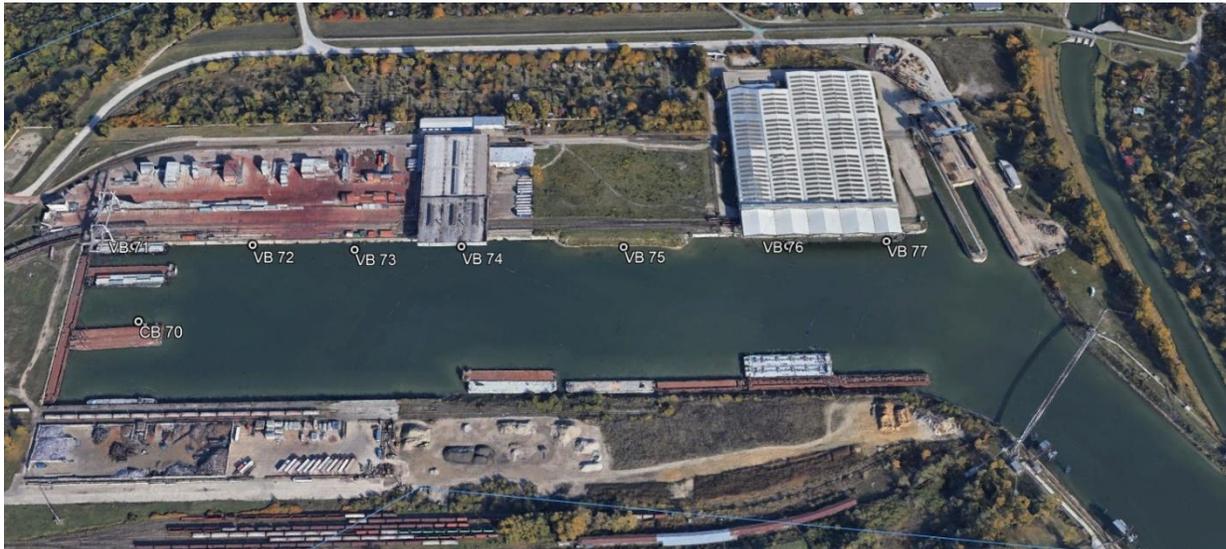
**Position EB 73** - trimodal handling position intended for vessels not carrying dangerous goods in length of 100 m, up to max. beam of 24 m.

**Position EB 74** - trimodal handling position intended for vessels not carrying dangerous goods in length of 100 m, up to max. beam of 24 m.

**Position EB 75** – parking position for berthing of vessels up to max. beam of 12 m in length of 100 m.

**Position EB 76-** handling position intended for transhipment provider vessels not carrying dangerous goods up to max. beam of 24 m in length of 100 m.

**Position EB 77-** handling position intended for metallurgy transhipment provider vessels not carrying dangerous goods up to max. beam of 24 m in length of 100 m



Source: VP, a.s.

**8.2.9 SECTION 8.- r.km 1865.400 - Pálenisko** – forms the east bank of the Pálenisko pool. It has three handling positions for heavy, oversized cargo and RO/RO loads. This section consists of the following positions:

**Position EB 80** – handling position intended for RO/RO vessels.

**Position EB 81** – handling position intended for vessels carrying heavy or oversized cargo

**Position EB 82** – handling position intended for vessels carrying heavy or oversized cargo - Priority berthing for crashed vessels. Berthing of crashed vessels is subject to instructions from the crisis staff



Source: VP, a.s.

**8.2.10 SECTION 9. - r.km 1865.400 - Pálenisko** – forms the west bank from the pipeline bridge over the Pálenisko pool mouth to the main flow of river Danube in length of 220 m. It has three handling positions for transshipment of liquid goods. This section consists of the following positions:

**Position DALBY 91** – handling position of liquid goods transshipment, where only one currently handled vessel can be made fast.

**Position DALBY 92** - handling position of liquid goods transshipment, where only one currently handled vessel can be made fast.

**Position DALBY 93** - handling position of liquid goods transshipment, where only one currently handled vessel can be made fast.



Source: VP, a.s.

**8.2.11 SECTION 10.- r.km 1865.000 – Ship repairs (NOL) –** forms the north bank of the port part of the Ship repairs pool from the front end to the mouth of the entrance, has eight parking and repair positions as follows:

**Position NB 101** – parking position for vessels which are under or expecting repairs up to max. beam of 70 m, in length of 85 m.

**Position NB 102** - parking position for vessels which are under or expecting repairs up to max. beam of 70 m, in length of 65 m.

**Position NB 103** - parking position for vessels which are under or expecting repairs up to max. beam of 70 m, in length of 70 m.

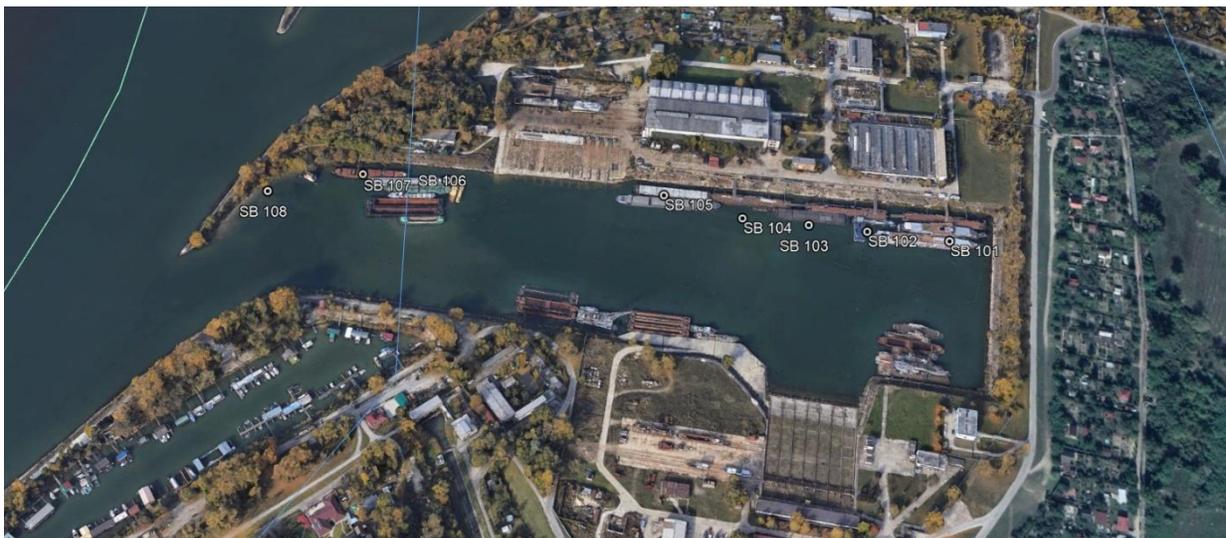
**Position NB 104** - parking position for vessels which are under or expecting repairs up to max. beam of 70 m, in length of 65 m.

**Position NB 105** - parking position for vessels which are under or expecting repairs up to max. beam of 70 m, in length of 80 m.

**Position NB 106** - parking position for vessels which are under or expecting repairs up to max. beam of 35 m, in length of 90 m.

**Position NB 107** - parking position for vessels which are under or expecting repairs up to max. beam of 35 m, in length of 90 m.

**Position NB 108** - parking position for vessels which are under or expecting repairs up to max. beam of 35 m, in length of 80 m.



Source: VP, a.s.

**8.2.12 SECTION 11. - r.km 1865.000 – Ship repairs (NOL) –** forms the south bank of the port area of the Ship repairs pool from the front bank to the mouth of the entrance area, has five parking and repair positions where the first three positions serve the vessels of the watercourse administrator, as follows:

**Position SB 111** – parking position up to max. beam of 35 m in length of 90 m.

**Position SB 112** - parking position up to max. beam of 25 m in length of 105 m.

**Position SB 113** - parking position up to max. beam of 35 m in length 105 m.

**Position SB 114** – parking position intended for berthing of vessels not carrying dangerous goods in the length of 95 m, up to max. beam of 25 m.

**Position SB 115** - parking position intended for berthing of vessels not carrying dangerous goods in the length of 95 m, up to max. beam of 25 m.



Source: VP, a.s.

**8.2.13 SECTION 12. - r.km 1864.600-1863.700 –** includes eight parking positions, four of which are located on the left bank, where vessels can be parked at a water level of up to 550 cm on the Bratislava water gauge and four positions on the right bank of the Danube River where vessels can be parked at a water level of 400 cm on the Bratislava water gauge.

**Position MFD 121L - left bank r.km 1864.600 to 1864.500** – parking position for vessels not carrying dangerous goods up to max. beam of 12 m, in length of 100 m long, where the bank vessel is anchored, made fast by two front bank ropes, a cross rope and a rear rope.

**Position MFD 122L - left bank r.km 1864.500 to 1864.400** - parking position for vessels not carrying dangerous goods up to max. beam of 12 m, in length of 100 m long, where the bank vessel is anchored, made fast by two front bank ropes, a cross rope and a rear rope.

**Position MFD 123L - left bank r.km 1864.400 to 1864.300** - parking position for vessels not carrying dangerous goods up to max. beam of 12 m, in length of 100 m long, where the bank vessel is anchored, made fast by two front bank ropes, a cross rope and a rear rope.

**Position MFD 124L - left bank r.km 1864.300 to 1864.200** - parking position for vessels carrying dangerous goods up to max. beam of 12 m, in length of 100 m long, where the bank vessel is anchored, made fast by two front bank ropes, a cross rope and a rear rope.

**Position MFD 128P- right bank r.km 1864.100 to 1864.000** – parking position intended for empty vessels not carrying any dangerous goods up to max. beam of 24 m in length of 100 m, with the bank vessel being anchored and made fast by one bank rope. With water level of 400 cm and higher in Bratislava the vessel will be made fast by two ropes.

**Position MFD 129P - right bank r.km 1864.000 to 1863.900** - parking position intended for empty vessels not carrying any dangerous goods up to max. beam of 24 m in length of 100 m, with the bank vessel being anchored and made fast by one bank rope. With water level of 400 cm and higher in Bratislava the vessel will be made fast by two ropes.

**Position MFD 130P - right bank r.km 1863.900 to 1863.800** - parking position intended for loaded vessels not carrying any dangerous goods up to max. beam of 24 m in length of 100 m, with the bank vessel being anchored and made fast with one front and one rear rope. With water level of 400 cm and higher in Bratislava also the second vessel will have to have a front bank rope.

**Position MFD 131P - right bank r.km 1863.800 to 1863.700** - parking position intended for loaded vessels not carrying any dangerous goods up to max. beam of 24 m in length of 100 m, with the bank vessel being anchored and made fast with one front and one rear rope. With water level of 400 cm and higher in Bratislava also the second vessel will have to have a front bank rope.



Source: VP, a.s.

## Article 9

### Komárno port

- 9.1** Public port of Komárno forms the left bank of the Danube River up to the left edge of the waterway in the section from r. km 1 770.000 to 1 762.000, both banks of the river Váh up to the railway bridge, bank with delimited territory. From the activities point of view, the port of Komárno is divided into six sections. The activities are focused on handling liquid goods, ferro-materials and bulk goods. The operator establishes a buffer zone under the bridges which intersect the Danube in the port aquatorium. This zone is 20 meters downstream and upstream for safety reasons. It is forbidden to make fast any FE in this location.
- 9.2** This part of the Port regulation provides for the handling, service, repair and parking positions, conditions and method of anchoring, making fast and admission of the vessel's berthing. The method of making fast is recommended and the making fast described at each position is minimal. Parking and handling positions of the public port of Komárno on the left bank of the free flow of river Danube include the following sections:

Komárno port							
Port section:	MFD	Passenger port	MFD	MFD	Eastern pool	Western pool	MFD
Section:	1.	2.		3.	4.	5.	6.
Number of port positions:	19	4	2	11	10	30	3
Position types:	Parking positions			Handling and parking positions			Parking positions

**9.2.1 SECTION 1.- r. km 1 770.000 to 1 768.100** has nineteen parking positions for vessels that not carrying any dangerous goods as follows:

**Position MFD 1 - left bank, r.km 1770.000 to 1769.900** parking position in length of 100 m for vessels not carrying any dangerous cargo where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored ad the third vessel being anchored.

**Position MFD 2 - left bank, r.km 1769.900 to 1769.800** parking position in length of 100 m for vessels not carrying any dangerous cargo where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored ad the third vessel being anchored.

**Position MFD 3 - left bank, r.km 1769.800 to 1769.700** parking position in length of 100 m for vessels not carrying any dangerous cargo where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored ad the third vessel being anchored.

**Position MFD 4 - left bank, r.km 1769.700 to 1769.600** parking position in length of 100 m for vessels not carrying any dangerous cargo where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored ad the third vessel being anchored.

**Position MFD 5 - left bank, r.km 1769.600 to 1769.500** parking position in length of 100 m for vessels not carrying any dangerous cargo where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 6 - left bank, r.km 1769.500 to 1769.400** parking position in length of 100 m for vessels not carrying any dangerous cargo where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 7 - left bank, r.km 1769.400 to 1769.300** parking position in length of 100 m where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 8 - left bank, r.km 1769.300 to 1769.200** parking position in length of 100 m where max. of three vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 9 - left bank, r.km 1769.200 to 1769.100** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 10 - left bank, r.km 1769.100 to 1769.000** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 11 - left bank, r.km 1769.000 to 1768.900** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 12 - left bank, r.km 1768.900 to 1768.800** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 13 - left bank, r.km 1768.800 to 1768.700** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 14 - left bank, r.km 1768.700 to 1768.600** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 15 - left bank, r.km 1768.600 to 1768.500** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 16 - left bank, r.km 1768.500 to 1768.400** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 17 - left bank, r.km 1768.400 to 1768.300** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 18 - left bank, r.km 1768.300 to 1768.200** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.

**Position MFD 19 - left bank, r.km 1768.200 to 1768.100** parking position in length of 100 m where max. of four vessels may be made fast, with the first vessel being made fast by the bank rope and anchored and the third vessel being anchored.



Source: VP, a.s.

**9.2.2 SECTION 2. - r. km 1 768.100 to 1 767.200** has six positions where the first four are intended for the FE for making fast of passenger vessels and the other two are for SPS controls

**Position PPKN 21 - left bank r.km 1768.100 - 1767.950** – is intended for floating establishment, which will be used for making fast of vessels up to a maximum beam of 24 m with max. 150 m in length while the first vessel will be anchored and made fast by the front and rear bank rope, the second vessel will be made fast by the front bank rope. The location and making fast of the FE will be in accordance with the berthing approval decision issued by SPS.

**Position PPKN 22 - left bank r.km 1767.940 - 1767.830** – is intended for floating establishment, which will be used for making fast of vessels up to a maximum beam of 24 m with max. 120 m in length while the first vessel will be anchored and made fast by the front and rear bank rope, the second vessel will be made fast by the front bank rope. The location and making fast of the FE will be in accordance with the berthing approval decision issued by SPS.

**Position PPKN 23 - left bank r.km 1767.780 - 1767.600** – is intended for floating establishment which will be used for making fast of vessels. The locations and making fast of the FE will be in accordance with the berthing approval decision issued by SPS.

**Position PPKN 24 - left bank r.km 1767.600 - 1767.470** - is intended for floating establishment, which will be used for making fast of vessels up to a maximum beam of 24 m with max. 120 m in length while the first vessel will be anchored and made fast by the front and rear bank rope, the second vessel will be made fast by the front bank rope.

**Position MFD 25 - left bank r.km 1767.470 - 1767.360** – is intended for floating establishment, which is used for making fast a max. of 3 vessels side by side up to max. beam of 38 m, with the first vessel being anchored and made fast by the front and rear bank rope, the second and third vessels must be anchored. Position serves to inspect vessels performed by the SPS.

**Position MFD 26 - left bank r.km 1767.360 - 1767.200** – is intended for possible inspections of freight vessels. If the SPS does not set otherwise, the vessels may be made fast up to max. beam of 50 m from the bank with a length of 150 m, each second vessel being anchored. After the inspection, the vessels must leave this position, the SPS may grant an exemption.



Source: VP, a.s.

**9.2.3 SECTION 3. - r. km 1 767.000 - 1766.000** – forms the north bank from the mouth of the eastern pool to the mouth of the river Váh and has ten parking and handling positions

**Position MFD 30 r.km 1767.000 om direction from Eastern pool** – handling position for vessels not carrying any dangerous goods where one vessel is allowed to park. The vessel will be made fast by a front, cross, and a rear rope and will be anchored.

**Position MFD 31 r.km 1767.000 - 1766.915** - handling position for vessels not carrying any dangerous goods where one vessel is allowed to park. The vessel will be made fast by a front, cross, and a rear rope and will be anchored.

**Position MFD 32 r.km 1766.915 - 1766.830** - handling position for vessels not carrying any dangerous goods where one vessel is allowed to park. The vessel will be made fast by a front, cross, and a rear rope and will be anchored.

**Position MFD 33 r.km 1766.830 - -1766.740** - handling position for vessels not carrying any dangerous goods where one vessel is allowed to park. The vessel will be made fast by a front, cross, and a rear rope and will be anchored.

**Position MFD 34 r.km 1766.740 -1766.650** – handling position for vessels that not carrying any dangerous goods where max. of three vessels are allowed to park with a beam of 38 m, with the first vessel made fast by a front, cross and rear bank rope. The first and third vessel will be anchored

**Position MFD 35 r.km 1766.600 - 1766.500** – handling position for vessels that not carrying any dangerous goods where max. of three vessels are allowed to park with a beam of 38 m and length of 150 m, with the first vessel made fast by a front, cross and rear bank rope. The first and third vessel will be anchored

**Position MFD 36 r.km 1766.500 - 1766.400** - handling position for vessels that not carrying any dangerous goods where max. of three vessels are allowed to park with a beam of 38 m, with the first vessel made fast by a front, cross and rear bank rope. The first and third vessel will be anchored.

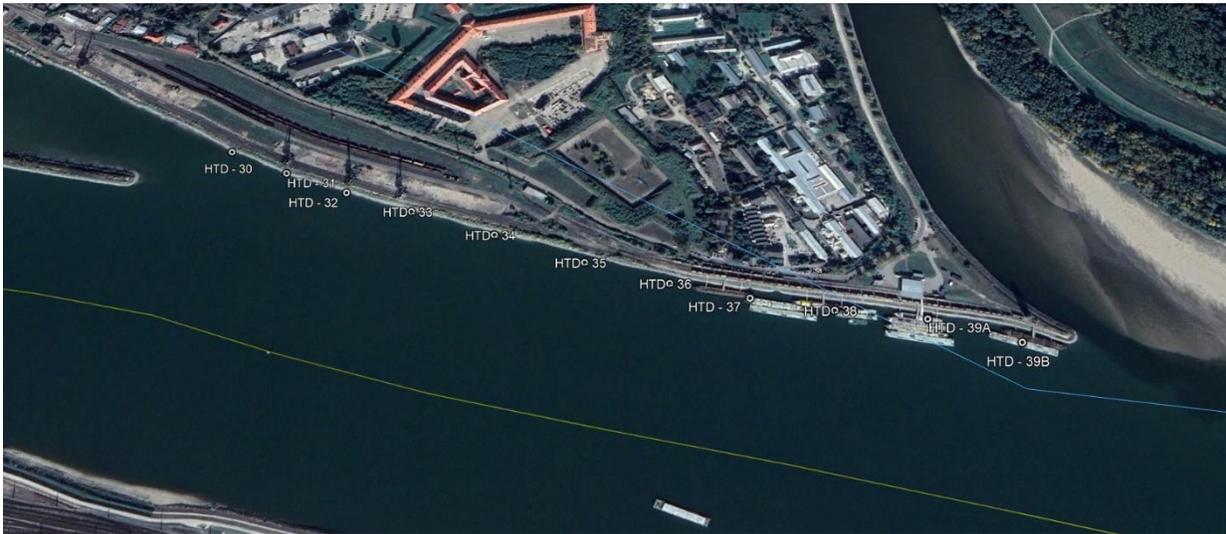
**Position MFD 37 r.km 1766.400 - 1766.300** – handling position on the pontoon, which will be made fast in accordance with the SPS decision and is intended for vessels carrying dangerous goods. On the pontoon three vessels up to max. beam of 38 m carrying dangerous goods may be made fast, if these are loaded the first and third vessel must be anchored, and the second vessel must be made fast by the bank rope.

**Position MFD 38 r.km 1766.300 - 1766.200** - handling position on the pontoon, which will be made fast in accordance with the SPS decision and is intended for vessels carrying dangerous goods. On the pontoon three vessels up to max. beam of 38 m carrying dangerous goods may be made fast, if these are loaded the first and third vessel must be anchored, and the second vessel must be made fast by the bank rope.

**Position MFD 39A r.km 1766.200 - 1766.100** - handling position on the pontoon, which will be made fast in accordance with the SPS decision and is intended for vessels carrying dangerous goods. On the pontoon three vessels up to max. beam of 38 m carrying dangerous goods may be made fast, if these are loaded the first and third vessel must be anchored, and the second vessel must be made fast by the bank rope.

**Position MFD 39B r.km 1766.100 - 1766.000** - handling position on the pontoon, which will be made fast in accordance with the SPS decision and is intended for vessels carrying dangerous goods. On the pontoon three

vessels up to max. beam of 38 m carrying dangerous goods may be made fast, if these are loaded the first and third vessel must be anchored, and the second vessel must be made fast by the bank rope.



Source: VP, a.s.

**9.2.4 SECTION 4. - r. km 1 767.100 – Eastern pool –** forms the north bank from the mouth of the eastern pool to the closing object and continues along the south bank to the mouth of the eastern pool as follows:

**Position NB 41** – handling position for vessels not carrying any dangerous goods where berthing of one vessel is permitted with length of 100 m. The vessel will be made fast by the front, cross, rear rope and anchored up to max. beam of 24 m.

**Position NB 42** – – handling position for vessels not carrying any dangerous goods where berthing of one vessel is permitted with length of 100 m. The vessel will be made fast by the front, cross, rear rope and anchored up to max. beam of 24 m.

**Position NB 43** – – handling position for vessels not carrying any dangerous goods where berthing of one vessel is permitted with length of 100 m. The vessel will be made fast by the front, cross, rear rope and anchored up to max. beam of 38 m.

**Position NB 44** – parking position for vessels not carrying any dangerous goods and intended for vessels with own propulsion. Max. beam 45 m with length of 85 m.

**Position NB 45** – parking position for vessels not carrying any dangerous goods and intended for the parking of vessels requiring maintenance or repair with max. beam of 38 m and 80 m in length.

**Position NB 46** - parking position for vessels not carrying any dangerous goods and intended for the parking of vessels requiring maintenance or repair with max. beam of 24 m and 80 m in length.

**Position NB 47** - parking position for vessels not carrying any dangerous goods and intended for the parking of vessels requiring maintenance or repair with max. beam of 24 m and 80 m in length.

**Position SB 48** – parking positions 250 m in length, the first part of position from the folding bridge edge is intended for the FE, which is used to make fast small vessels. The location and making fast of the FE must be done in accordance with berthing approval decision issued by the SPS. The second part of the position is intended for vessels not carrying any dangerous goods up to max. beam of 24 m.

**Position SB 49A** – parking position 60 m in length is intended exclusively for service vessels of the SPS.

**Position SB 49B** – parking position of the Operator from the east pool mouth of the south bank towards the closing object 190 m in length and is intended for vessels carrying dangerous goods only in the case of unfavourable navigation conditions.



Source: VP, a.s.

**9.2.5 SECTION 5. - r. km 1 767.100 – western pool** – forms the northern bank from the closing object towards the front bank and continues with the south bank towards the closing object. The making fast of vessels in individual positions is identical as follows: the first vessel made fast must have one front, rear and cross rope, the second vessel will have one front and rear rope in order to prevent the vessel from moving in both axis of the vessel

**Position NB 5.1** – a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.2** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.3** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.4** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.5** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.6** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.7** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.8** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.9** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.10** - a handling position intended for vessels not carrying any dangerous goods, with a maximum capacity of two vessels up to max. beam of 24 m in length of 100 m.

**Position NB 5.11** – parking position in length of 100 m for floating establishments used for berthing of vessels not carrying any dangerous goods.

**Position NB 5.12** - parking position in length of 100 m for floating establishments used for berthing of vessels not carrying any dangerous goods.

**Position NB 5.13** – purpose-built position for the operator of Komárno shipyard in the length of 820 m for the stay of vessels up to max. beam of 50 m. Testing of machinery and propulsion equipment (start-up) may only be carried out in a defined area under the license and conditions of the SPS. Starting of the boat lift in winter is only allowed after the SPS notification and after the disruption of a continuous ice layer.

**Position SB 5.14** – parking position 150 m in length is intended for floating establishment, which is used to make fast small vessels and yachts in parking boxes of the floating establishment with max. beam of 50 m

**Position SB 5.15** – parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.16** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.17** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.18** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.19** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.20** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.21** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.22** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.23** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.24** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.25** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

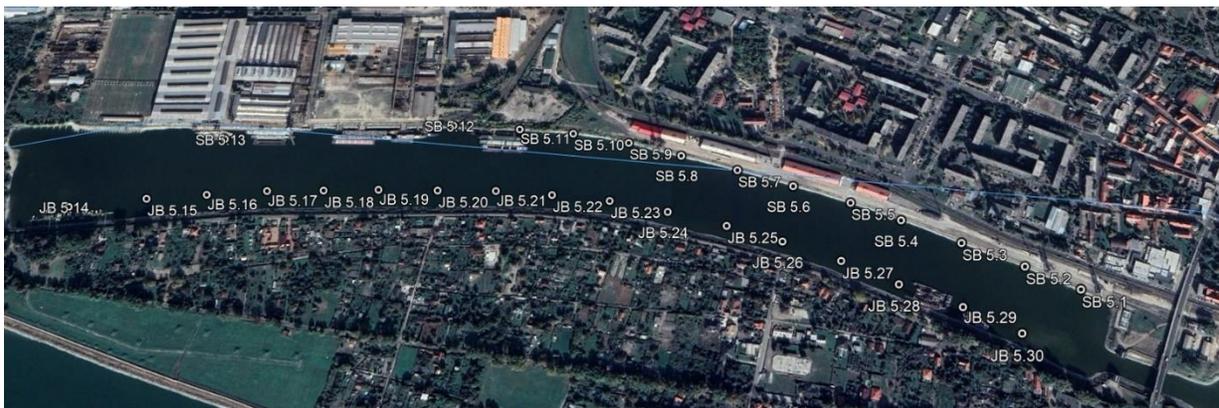
**Position SB 5.26** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.27** - parking position intended for parking of vessels not carrying any dangerous goods, with maximum capacity five vessels being made fast with max. beam of 60 m and 100 m in length.

**Position SB 5.28** – parking position intended for parking of vessels of the waterway administrator for technological vessels with max. beam of 40 m and 100 m in length.

**Position SB 5.29** - parking position intended for parking of vessels of the waterway administrator for technological vessels with max. beam of 40 m and 100 m in length.

**Position SB 5.30** - parking position intended for parking of vessels of the waterway administrator for technological vessels with max. beam of 40 m and 100 m in length.



Source: VP, a.s.

#### 9.2.6 SECTION 6. - r. km 1 764.000 – 1762.000 reserved for waiting and anchoring of vessels

**Position MFD 61 r.km 1764.000 – 1762.500** – anchorage for vessels not carrying any dangerous goods and may anchor up to max. beam of 150 m. This location is in the allocated area of the public port of Komárno and is subject to reporting obligations.

**Position MFD 62 r.km 1762.500 – 1762.000** – anchorage for vessels not carrying any dangerous goods and may anchor up to max. beam of 150 m. This location is in the allocated area of the public port of Komárno and is subject to reporting obligations.



Source: VP, a.s.

**Position no. 71 ( 6. SECTION ) - right bank** of the Váh river, a position of 80 m in length not specified is intended to accommodate a floating establishment. The location and making fast of the floating establishment shall be in accordance with the berthing approval decision issued by the SPS.



Source: VP, a.s.

## Article 10

### Štúrovo port

**10.1** Public port of Štúrovo forms the left bank of the Danube River up to the left edge of the waterway in the section from r. km 1 718.800 to 1 718.300 with delimited territory. The method of making fast is recommended and the making fast described at each position is minimal. The operator establishes a buffer zone under the bridges which intersect the Danube in the port aquatorium. This zone is 20 meters downstream and upstream for safety reasons. It is forbidden to make fast any FE in this location

Port section:	Passenger port
Section:	1.
Number of port positions:	6
Type of positions:	Parking positions

**10.2** Štúrovo public port consists of the following positions:

**Position PPŠT 1 r.km 1718.785 - 1718.760** – is intended for floating establishment, which will be used to make fast small vessels up to max. beam of 10 m.

**Position PPŠT 2 r.km 1718.750 - r.km 1718.730** – is intended for the FE, which will serve to make fast small vessels of the Customs administration up to max. beam of 10 m.

**Position PPŠT 3 r.km 1718.680 - 1718.580** – intended for floating establishment which will be used to make fast passenger ships up to a maximum length of 80 meters and a maximum beam of 38 meters, with the first vessel being anchored and made fast by a front and rear bank rope, the second being made fast by a front bank rope, and in the event of a third vessel parked, the third vessel being anchored. These methods of making fast do not apply to vessels up to 40 m in length. The placement and making fast of the FE will be in accordance with the SPS berthing approval decision.

**Position PPŠT 4 r.km 1718.580 - 1718.450** – intended for FE which will be used to make fast passenger ships up to a maximum length of 120 meters and a maximum beam of 38 meters, with the first vessel being anchored and made fast by a front and rear bank rope, the second being made fast by a front bank rope, and in the event of a third vessel parked, the third vessel being anchored. These methods of making fast do not apply to vessels up to 40 m in length. The placement and making fast of the FE will be in accordance with the SPS berthing approval decision.

**Position PPŠT 5 r.km 1718.440 - 1718.370** - intended for FE which will be used to make fast passenger ships up to a maximum length of 70 meters and a maximum beam of 24 meters, with the first vessel being anchored and made fast by a front and rear bank rope, the second being made fast by a front bank rope. These methods of making fast do not apply to vessels up to 40 m in length. The placement and making fast of the FE will be in accordance with the SPS berthing approval decision

**Position PPŠT 6 r.km 1718.360 - 1718.300** – is intended for floating establishment, which will serve to make fast passenger ships up to max. length of 120 m with a max. beam of 24 m.



Source: VP, a.s.

# PORT REGULATION – PART THREE

## Article 11

### Common and final provisions

- 11.1 The user of a public port providing handling, servicing or other services to other port users, operators or vessel owners as part of the inland waterway transportation shall be obliged to notify them of this port regulation.
- 11.2 In addition to this port regulation, users of the public port are obliged to comply with the Inland Navigation Act, the relevant provisions of Act no. 364/2004 Coll. (Water Act), as well as other related generally binding legal regulations of the Slovak Republic.
- 11.3 The currently valid navigation measure issued by the Transport Authority, including the Annex specifying the SPS conditions to ensure the safety of navigation in public ports on the Danube forms an integral part of the port regulation.
- 11.4 Landing of vessels is only possible after a thorough assessment of nautical conditions at port location. The port operator shall be obliged to inform the vessel owner of the situation.
- 11.5 At the same time, this port regulations constitutes the General Business Terms and Conditions of the Operator, applicable to users of public ports, and shall from an integral part of the Operator's instructions in accordance with the provisions of Article 5 par. 8 of the Inland Navigation Act.

## Article 12

### Transitional provisions

- 12.1 The rights and obligations resulting from previous authorizations and exemptions remain in force unless other legally binding regulation, with higher legal force, does not stipulate otherwise.

## Article 13

### Validity and effective date

- 13.1 The port regulation, which lay down the rules of port operations for public ports of the SR, shall enter into force after its approval by the Board of Directors of the Company and on the date on which it is published on the website of the Public Ports Operator, becoming effective on **15.03.2019**.
- 13.2 Location of the port regulation on the website of the Public Port Operator of the Slovak Republic:  
<http://www.portslovakia.com/sluzby/prevadzkovy-poriadok/>

in Bratislava, Date: 01.02.2019

Issued by: Verejné prístavy, a. s.

\_\_\_\_\_  
signature  
Ing. Gabriel Szekeres  
Chairman of the Board

\_\_\_\_\_  
signature  
Ing. Jozef Bódi  
Member of the Board

\_\_\_\_\_  
signature  
Ing. Emil Kosiba  
Member of the Board

### **Explanations and abbreviations:**

VP, a.s. - Verejné prístavy, akciová spoločnosť

SPS - State Professional Supervision

TA IWTD – Transport Authority – Inland Waterway Transport Division

PPBA – passenger port Bratislava

PPKN - passenger port Komárno

PPŠT - passenger port Štúrovo

FE – floating establishment

SR -Slovak Republic

NB – north bank

SB – south bank

WB – west bank

EB – east bank

FB – front bank

MFD – main flow Danube